

Fair Haven Borough Council

Special Meeting

September 8, 2021



Improvements to Hance Road and Cooney Terrace – Phase II

Borough of Fair Haven

NJDOT Municipal Aid – FY 2020

Theresa Casagrande, MBA, RMC, CTC, Past President NJMMA
Borough Administrator

- Borough Administrator – Borough of Fair Haven
- Master's Degree in Business Administration
- New Jersey Registered Municipal Clerk License
- New Jersey Certified Tax Collector License
- 2018 President of New Jersey Municipal Management Association
- Over 20 years of experience in Municipal Government
- Over 10 years with the Borough of Fair Haven




Capital Improvement Planning

- Monitoring Resources
- Monitoring Needs
- Long Range Planning for Spending
- Interdepartmental Coordination
- Contingencies for Emergency Capital Expenditures
- Debt Planning and Management - Tax Rate Impact
- Debt Authorization – Bond Ordinance
- Debt Issuance

Capital Improvement Process

- o Initial Planning
- o Governing Body Input
- o Committee Input
- o Professional Input
- o Committee Review
- o Professional Design
- o Professional Revisions – As needed
- o Additional Committee Review – As needed
- o Governing Body Presentation
- o Bid Process
 - Authorization to Bid
 - Bid Award
- o Project Timeline



Capital Improvement Financing

- o Bond Ordinance Authorization Process
 - Input from Department Heads
 - Input from Governing Body Members
 - Fair Haven Finance Committee
 - Professional Consultation
 - *Administrator*
 - *Chief Financial Officer*
 - *Financial Planner*
 - *Bond Counsel*
- o Bond Ordinance Adoption Process
 - Introduction
 - Publication
 - Public Hearing
 - Adoption

Grant Process Overview

- Planning
- Application
- Award
- Interface with Capital Planning
- Interface with Bond Ordinance Authorization
 - Grants are Specific to Projects
 - Bond Ordinances Tied to Grants are Specific to Projects
- Project Authorization
- Bid Process Implementation



Borough of Fair Haven

10 Year Grant History

2011 - 2021

Fair Haven Grant Total - \$4,819,279.00

NJ Department of Transportation (NJDOT) Grant Total - \$2,886,000.00

Funded through the NJ Transportation Trust Fund – Gasoline Tax

Borough of Fair Haven Road Project Overview

- Battin Road – Nearly Complete
- Harrison Avenue – Ongoing
- Willow Street – Ongoing
- Hance Road and Cooney Terrace – In Queue
- Various Drainage Improvements – In Queue
- Various Sidewalk Repairs and ADA Compliance Projects – In Queue
- Hunting Lane – In Queue
- River Road West Streetscape – On Hold due to NJAW Project
- Third Street Phase II – In Queue
- Fair Haven Road Phase I – NJDOT Grant Application Submitted

Richard Gardella, PE, PP, CME, CPWM

Director of Engineering and Public Works

- Associate in Applied Science Degree – Civil/Construction Engineering Technology
- Bachelor of Science Degree – Civil Engineering Technology
- Master Degree - City and Regional Planning
- Licensed Professional Engineer
- Licensed Professional Planner
- Certified Municipal Engineer
- Certified Public Works Manager
- Over 25 years experience - Municipal Engineering and Planning
- Nearly 14 years with the Borough of Fair Haven

Nicolas Poruchynsky, PE, PP, CME, CPWM, CFM

Assistant Director of Engineering and Public Works

- Bachelor of Science Degree – Civil Engineering
- Licensed Professional Engineer
- Licensed Professional Planner
- Certified Municipal Engineer
- Certified Public Works Manager
- Certified Flood Plain Manager
- Nearly 25 years experience - Municipal Engineering and Planning
- Nearly 10 years with the Borough of Fair Haven



Element of a Larger Plan



River Road
Streetscape
Improvements



Traffic Signal
Upgrade - River
Road and Hance
Road



Improvements to
Hance Road – Ridge
Road to River Road
and River Road to
Lewis Point Road



Future Waterfront
Park improvements



River Road
Resurfacing,
including Ped/Bike
improvements

Project Selection History

2006 - Initial phase of Hance Road improvements, from Ridge Road to River Road.

2014 – Governing Body authorized submission of NJDOT Municipal Aid grant for Improvements to Hance Road, from River Road to Cooney Terrace. Scope of work included: milling/paving, concrete curb (both sides), sidewalk (one side), ADA curb ramps, and drainage improvements. ***Project not selected by NJDOT.***

2016 – Governing Body authorized design and bid for Improvements to Hance Road, from River Road to Lewis Point Road. ***Project completed as part of 2017 Road and Sidewalk Program.***

2019 – Governing Body authorized submission of NJDOT Municipal Aid grant for Improvements to Hance Road – Phase II, from Lewis Point Road, including Cooney Terrace. Scope of work included: milling/paving, concrete curb (both sides), sidewalk (one side), ADA curb ramps, and drainage improvements. ***Project selected with deadline to award bid by 11/21/2021. Grant amount of \$350,000 – largest NJDOT Municipal Aid awarded to Fair Haven.***

2020 - Governing Body authorizes \$1,386,000 in Debt by adopting Bond Ordinance #2020-12, which includes \$600,000.00 total for the Hance Road and Cooney Terrace Project, with offsetting revenue totaling \$350,000.00 in an NJDOT Grant.

Design Regulations and Guidance

AASHTO – American
Association of State
Highway and
Transportation Officials

RSIS – Residential Site
Improvement Standards
(NJAC 5:21)

Americans with
Disabilities Act (ADA)

Fair Haven Master Plan

Fair Haven Active
Transportation Plan
(ATP)

NJDOT Complete Streets

Borough Sidewalk
Assessment Study

Prior Subdivision
Approvals

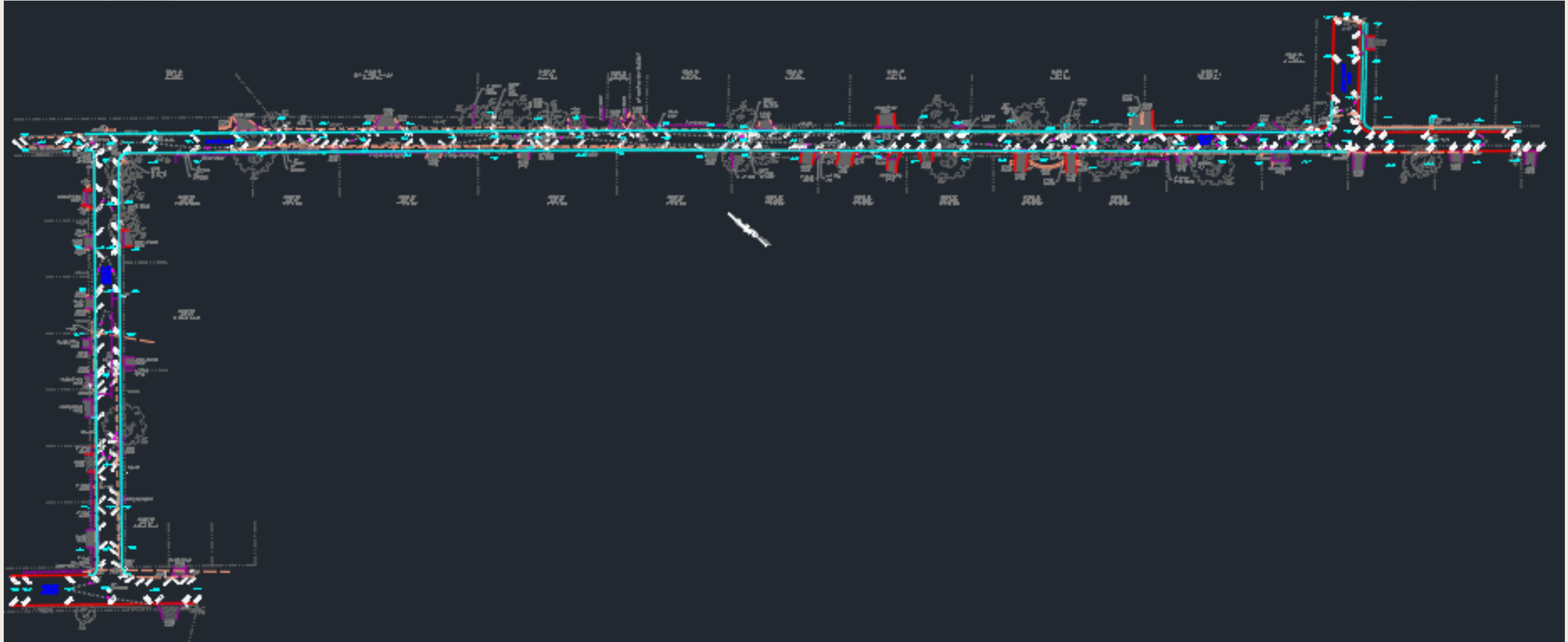
Monmouth County,
Rutgers, and Other

Existing Conditions Hance Road and Cooney Terrace

- Approximately 2,000 LF
- Limited Right-of-Way (ROW)
- Varying and narrow cartway width
- Poor pavement condition
- Erosion along edge of pavement
- No sidewalks or ADA compliance
- Sections of concrete and asphalt curb
- Utility pole conflicts
- Existing trees
- Drainage infrastructure
- Encroachments
- Speed limit – 25 mph
- Parking restrictions – No parking west side
- Residential properties - 30

BaseMap

Hance Road and Cooney Terrace





Project Goal

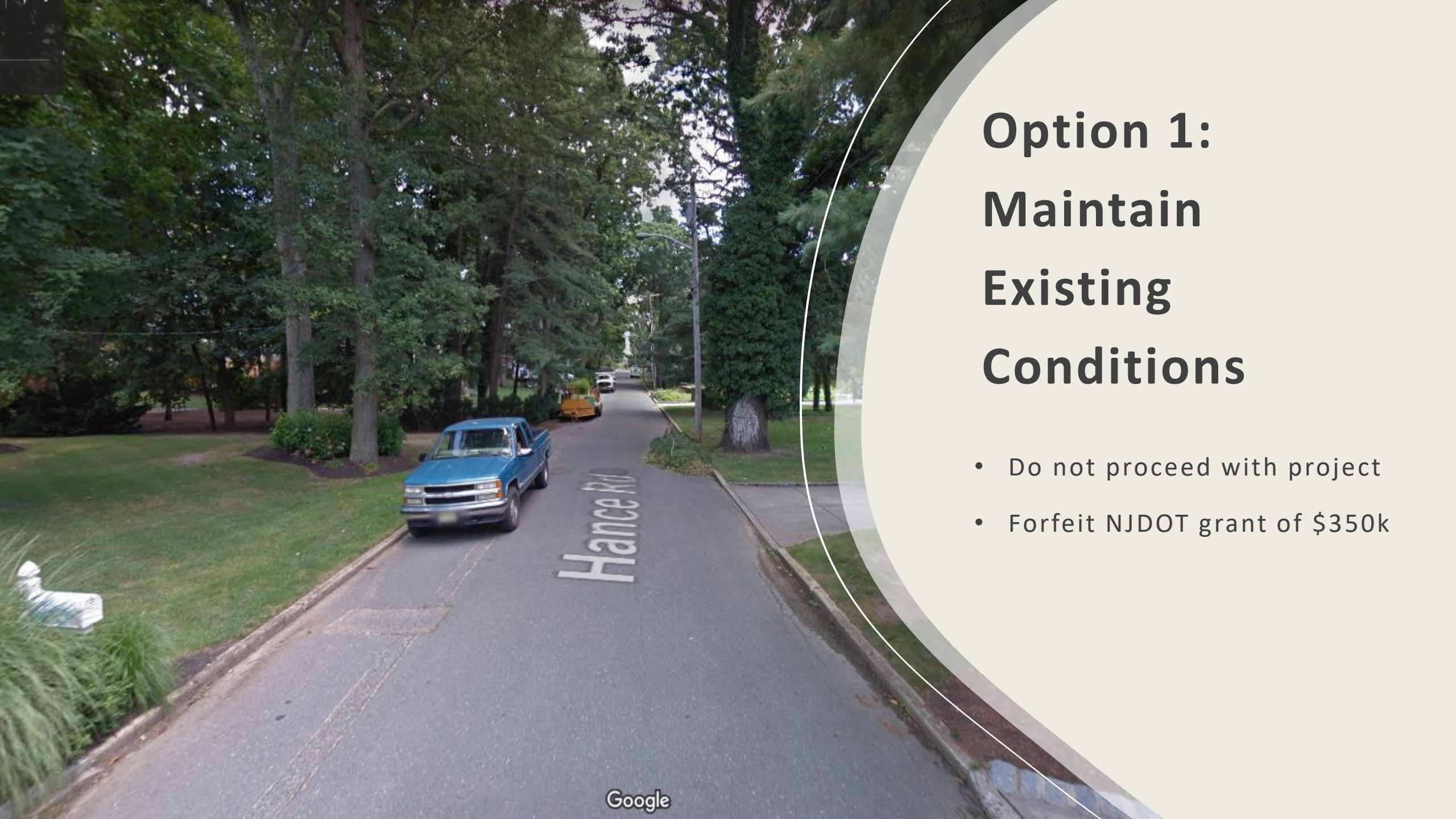
Improve the condition of roadway infrastructure while preserving the character of the neighborhood



Design Options

Traditional and Multimodal

1. Maintain Existing Conditions
2. Mixed Traffic - Yield Roadway
3. Physically Separated – Sidewalks
4. Mixed Traffic - Bicycle Boulevard
5. Visually Separated - Pedestrian Lane



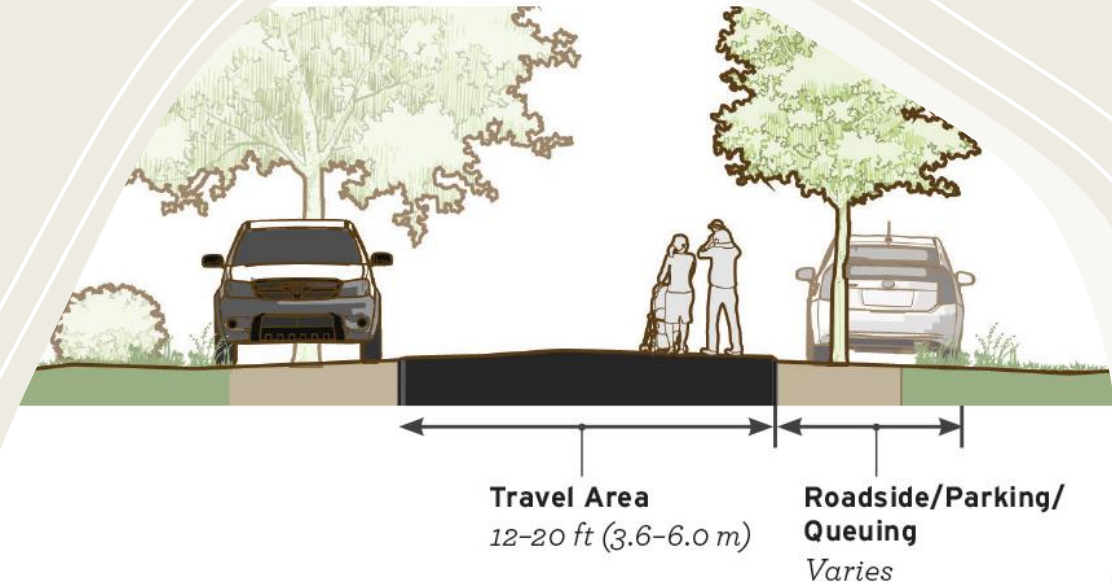
Option 1: Maintain Existing Conditions

- Do not proceed with project
- Forfeit NJDOT grant of \$350k

Option 2:

Mixed Traffic – Yield Roadway

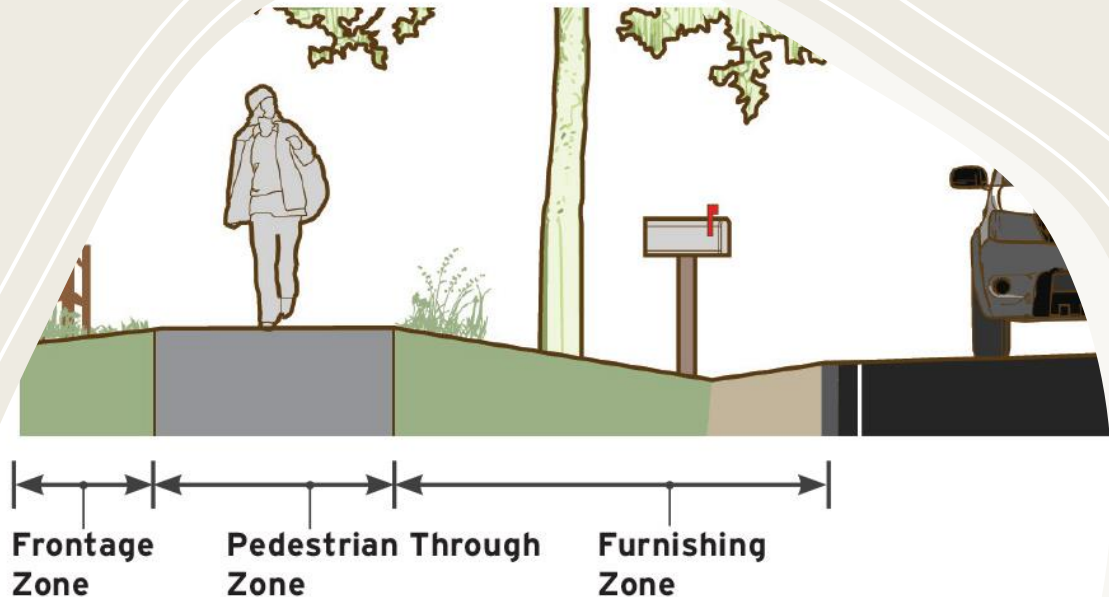
- Hance Road – widen to 22 feet
- Cooney Terrace – maintain 28 feet width
- Roadside/Parking/Queuing Treatment
- Curb (both sides)
- Pedestrian signage
- Prohibit parking in advance of intersections
- Provide adequate stopping sight distance at intersections (twice the normal distance)



Option 3:

Physically Separated – Sidewalk

- Hance Road – widen to 22 feet
- Cooney Terrace – maintain 28 feet width
- Curb (both sides)
- Sidewalk (east side) to be accessible by all users
- Buffer (grass strip) between curb and sidewalk
- Marked crosswalks at intersections
- Prohibit parking in advance of intersections
- Provide adequate stopping sight distance at intersections

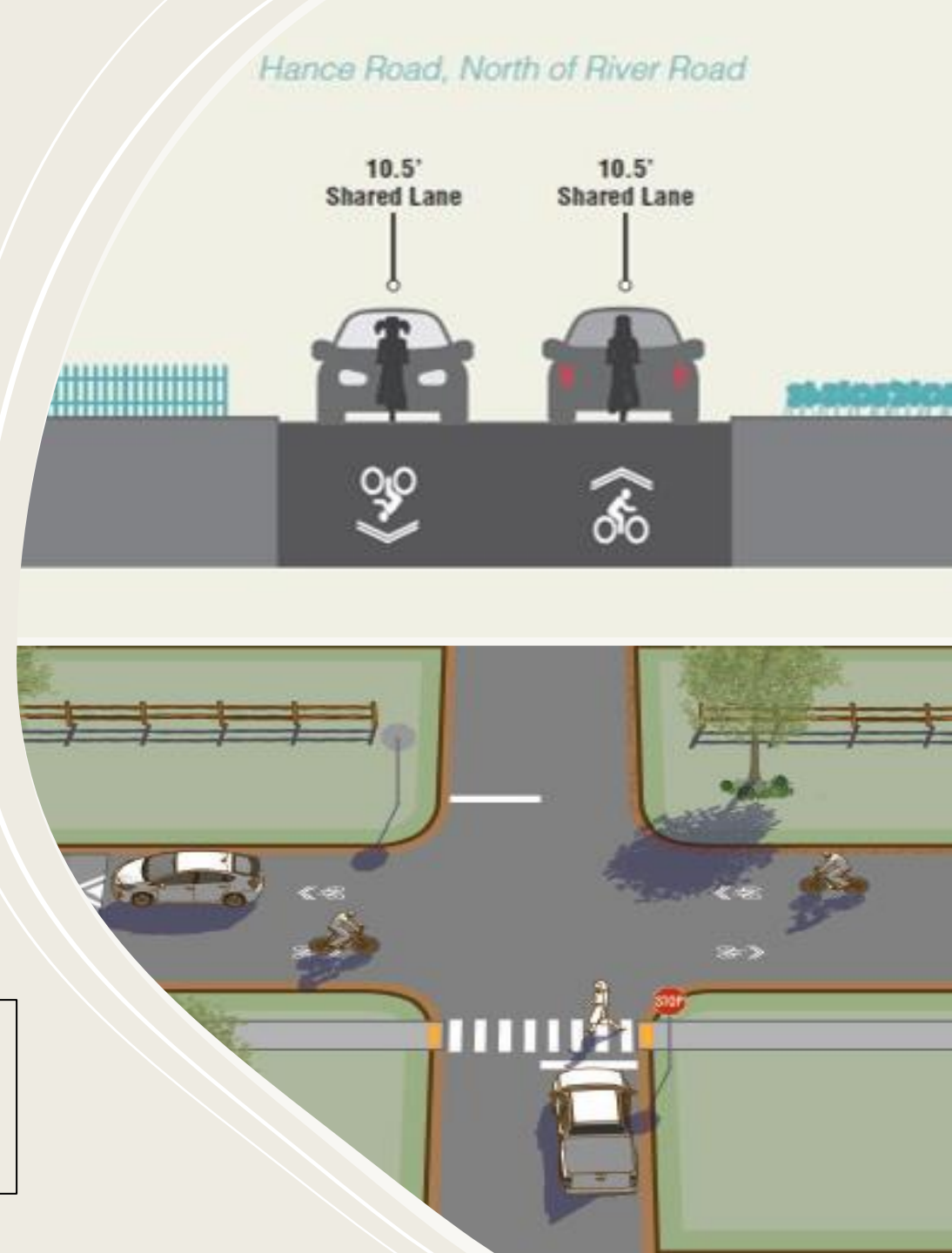


Option 4:

Mixed Traffic – Bicycle Boulevard

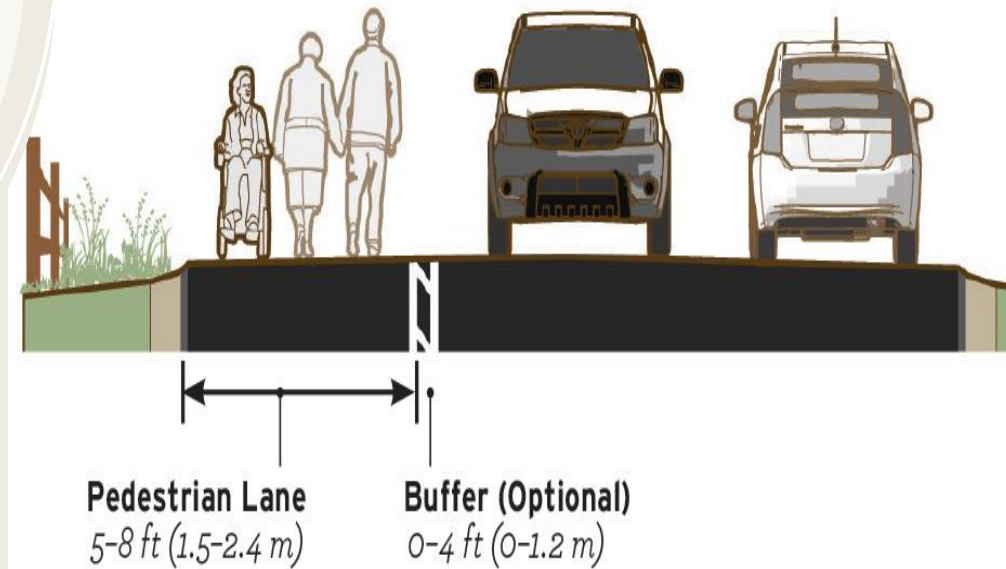
- Hance Road – widen to 22 feet
- Cooney Terrace – maintain 28 feet width
- Install curb (both sides)
- Install sidewalk (east side) to be accessible by all users
- Provide a buffer (grass strip) between curb and sidewalk
- Marked sharrows in travel lane
- Marked crosswalks at intersections
- Prohibit parking – entire length
- Provide adequate stopping sight distance at intersections

Bicycle Boulevards are designed to prioritize use by bicyclists and are not intended for use by pedestrians. On bicycle boulevards, the appropriate pedestrian facility is a sidewalk.



Option 5: Visually Separated - Pedestrian Lane

- Hance Road – widen to 28 feet
- Cooney Terrace – maintain 28 feet width
- Pedestrian Lane – Min. 5 feet wide with markings
- Curb (both sides)
- Buffer with striping
- Prohibit parking – entire length with signage
- Provide adequate stopping sight distance at intersections





Questions/Comments

-Governing Body Members

-Public Participation

*Images in this presentation from: Google Maps, Fair Haven ATP, and
Small Town and Rural Design Guide